

Our Ref : LTA/DBC/R70.018.001

Date : 1 March 2023

Who should know

Building Owners, Developers, Architects, Engineers, Transport/Traffic Consultants and Builders

LODGEMENT SCHEME EXPANDED TO ALLOW LODGEMENT OF LAYOUT PLAN FOR FARM DEVELOPMENTS

Effective date

1 March 2023

1. The Land Transport Authority (LTA) continually reviews our policies and processes to simplify the clearance of development proposals.
2. In April 2004, LTA first introduced the lodgement scheme for Street Plan for landed dwelling house developments not affected by road reserve lines. Over the years, LTA steadily increased the coverage of the lodgement scheme for Street Plan to include all development types other than for commercial developments and mixed-use developments.
3. As a further expansion of the lodgement scheme, we are pleased to announce that LTA will now accept lodgement of **farm developments**¹ at the Layout Plan (Development Control) stage. To qualify for lodgement, the QP shall prepare the layout plans and declare compliance with LTA's requirements stipulated in the prevailing codes of practice, standards and guidelines. After lodging the proposal with LTA, QP can proceed with the work without waiting for LTA's approval subject to any other clearances required from other authorities.
4. For development proposals located within the Railway Protection Zone, QPs can lodge the Layout Plan with LTA's Planning Compliance Division after clearing the proposal with LTA's Infrastructure Protection Division.
5. QPs are advised to synchronise your ESPro to download the latest forms (Version 1.1) from the Form Templates for use. Any applications submitted with the old forms will not be processed.

6. For more information on the lodgement scheme, QPs can refer to the quick guide on layout (development control) plans via the link [LTA | Journals & Newsletters](#) or by scanning the QR code below.



Dissemination & Feedback

7. We would greatly appreciate your assistance in disseminating this circular to members of your organisation and to others who may find this information useful. We can be reached at LTA_DBC_Registry@lta.gov.sg for any feedback.

Chan Chin Kai
Director
Development & Building Control

¹ All farm developments are eligible for lodgement unless they involve works listed below

- a) developments affected by Road Reserve Line.
- b) developments with proposed accesses that are within a cul de sac/dead-end road.
- c) proposals involving construction of a new street/widened street or requiring change in traffic scheme (including proposed relocation of hump, traffic signs and bus stops).
- d) proposals involving relocation of access to another road frontage.
- e) existing encroachment of the road reserve.
- f) proposed works within the Road Structure Safety Zone (RSSZ).
- g) proposed works that are carried out within 50 metres from the line of road tunnel reserve.



Lodgement Submission for Layout (Development Control) Plans

For
*Landed Dwelling House
& Farm Developments*



WHAT IS LTA'S LODGEMENT SCHEME?

LTA's lodgement scheme is a self-declaration scheme wherein proposed development works are checked and declared by a Qualified Person (QP) to be in full compliance with LTA's prevailing code of practice, standards and guidelines. Upon successful submission, the QP will receive an acknowledgement of lodgement from LTA.

This quick guide focuses on the lodgement of Layout Plan submissions, for developments not located within the Railway Protection Zone.

Should the development proposal fall within the Railway Protection Zone, a **separate submission** shall be made to LTA for clearance under the Rapid Transit Systems Regulations,

WHAT PROPOSALS CAN BE LODGED WITH LTA AT THE LAYOUT PLAN (DEVELOPMENT CONTROL) STAGE?

Currently, Layout (Development Control) Plans for **Landed Residential** and **Farm** developments qualify for LTA's lodgement scheme. Additionally, the proposal should satisfy the following criteria:

- The proposed development plot(s) is/are not affected by Road Reserve Lines.
- The vehicular access is not relocated to another frontage.
- There are no proposed roads to be constructed.
- There are no changes to the traffic scheme (e.g. proposed external works do not include relocation of humps, bus stop, road marking, traffic signs etc.).
- There are no existing/new encroachments of the road reserve.
- There are no works carried out within the road structure safety zone.
- There are no works that are carried out within 50m from the line of tunnel reserve.
- For landed residential developments: the proposal does not comprise more than 2 dwelling units and each landed dwelling house has its own parking lot.

HOW TO MAKE A LODGEMENT SUBMISSION TO LTA

- The QP shall lodge the Layout Plans via CORENET ESS.
- The e-Form 'LTA-DBC_Roads.xfdx' consists of 5 sections. Please carry out the following steps:
 - Read the '*Instructions*'.
 - Under '*Submission Type*', select 'New Submission', indicate 'Application Type 1' under 'Application Type', and further indicate
 - 'YES' - If there are proposed external works within the road reserve.
 - 'NO' - If there are no proposed external works within the road reserve.
 - Under '*Application Information*', fill in the details of your application. Please ensure that your development is either a landed residential or farm development.
 - Fill in your particulars under '*Particulars of Sender*'.
 - Declare all mandatory conditions under '*Declaration*'. Please ensure that your proposal adheres to the criteria stipulated above, as well as the other conditions within the declaration section.

The screenshot displays the 'APPLICATION FOR ROADS' web interface. On the left, a sidebar contains five numbered sections: 1. Instructions, 2. Submission and Application Type, 3. Application Information, 4. Particulars of Sender, and 5. Declaration (highlighted in red). The main content area is titled 'Lodgement for Approval of Layout Plans' and shows 'Section IIA Declaration of Conditions'. It contains a list of conditions with checkboxes for selection. Condition 1.1 is selected. Below the conditions, there are radio buttons for 'Submission for Landed Dwelling House' and 'Submission for Farm Development'. At the bottom, there are 'Save' and 'Previous' buttons. The footer indicates 'Last Updated Date: -- LTA-DBC-ROADS -- Ver. 1.0'.

DOCUMENTS TO SUBMIT

In your Layout Plan lodgement submission to LTA, you will need to furnish the following documents:

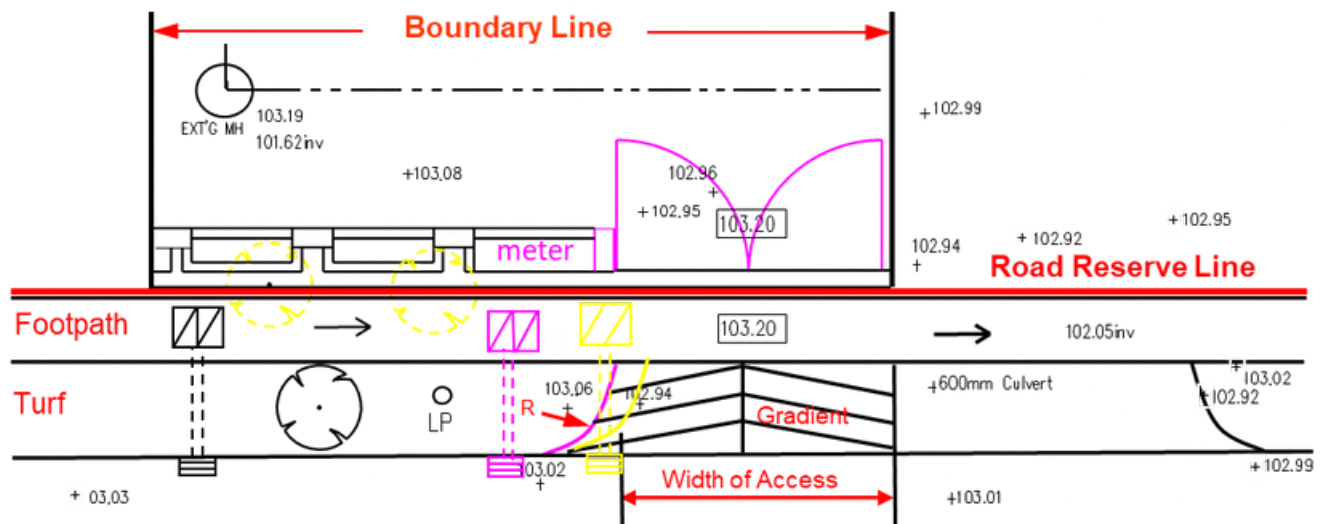
- Completed application form "LTA_DBC_Road.xfdx"
- One set of layout plans including:
 - Site plan
 - Floor plans
 - Section plans (including road features)
 - Elevation plans
 - Topographical plan
- Latest site photos (with date stamp) showing the existing frontage of landed dwelling house / farm development. Date of photos taken shall not be older than 1 month from date of submission.

Please ensure that all of the above documents are included in your submission.

DETAILS TO SHOW ON THE SITE PLAN

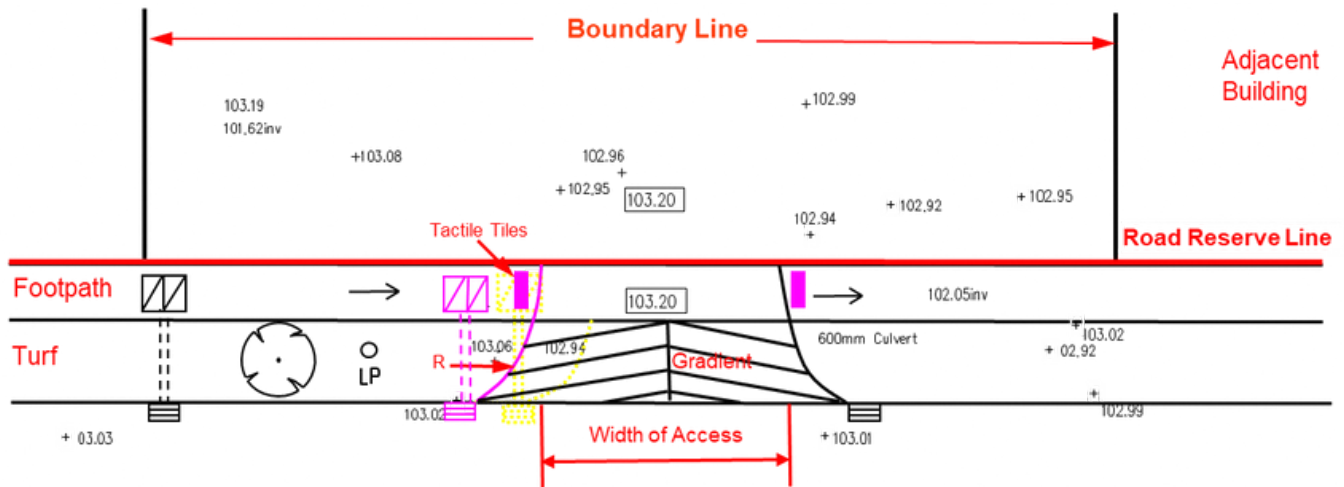
It is important to clearly indicate the proposed street works that will be carried out in your submission. Please ensure that the following details are clearly presented in your site plan:

FOR LANDED RESIDENTIAL DEVELOPMENTS



- Show road features to be deleted in dotted yellow lines, proposed road features in magenta, existing in cyan.
- Indicate road reserve line in bold red.
- Indicate width of proposed access and footpath.
- Indicate proposed turning kerb radius.
- Indicate gradient of proposed entrance approach.
- Indicate existing road levels, proposed culvert & internal platform levels.
- Proposed top level of culvert to be higher than road edge level.
- Show location of meter compartment.
- Show proposed location of lamp post affected by proposal, if any.
- Show proposed drop inlet chamber at tangent points of turning radius.
- Show position of pedestrian side gate, if any.
- Annotate on plan whether the existing culvert is to be retained or any other proposed works within the road reserve.

FOR FARM DEVELOPMENTS



- Show road features to be deleted in dotted yellow lines, proposed road features in magenta, existing in cyan.
- Indicate road reserve line in bold red.
- Indicate width of proposed access and footpath.
- Indicate proposed turning kurb radius.
- Indicate gradient of proposed entrance approach.
- Indicate existing road levels, proposed culvert & internal platform levels.
- Proposed top level of culvert to be higher than road edge level.
- Show proposed location of lamp post affected by proposal, if any.
- Show proposed 600mm wide yellow homogenous tactile tiles at 300mm from the kerb line on both sides of the entrance on footpath. (please refer to quick guide on "Designing Tactile Indicators for Safe Travel").
- Show proposed drop inlet chamber at tangent points of turning radius.
- Show position of pedestrian side gate, if any.
- Annotate on plan whether the existing culvert is to be retained or any other proposed works within the road reserve.

COMMON MISTAKES

Over the years, LTA has noted several common errors made by industry professionals when lodging layout plans. They include the following:

1. **Turning kerbs which are not in line with the gate post.**
2. **Improper or non-provision of Type 'B' aluminium alloy railing.**
3. **Concrete finishings on top of culvert.**
4. **Improper levelling of culvert and access resulting in water stagnation issues.**



Turning kerb is to be in-line with gate post to prevent vehicles from colliding with the gate post or mounting over the kerb and road sidetable



Type 'B' aluminium alloy railing to be provided next to existing open drain to prevent falling into the open drain



Top of culvert shall be in concrete finish to facilitate road re-surfacing works and for pedestrian connectivity



Top of culvert shall be levelled correctly to prevent water stagnation



COMMON MISTAKES (CONTINUED)

5. Encroachments of the road reserve.
6. Sliding gates between the boundary wall and the road reserve, posing as tripping hazard.
7. Improper or non-provision of drop inlet chambers.
8. Top-of-culvert levels sloping towards the road carriageway.



No encroachment within road reserve



Sliding gate shall be placed behind the boundary wall to prevent tripping hazard



Drop in-let chamber shall be provided at the tangent point of the turning kerb to ensure surface water runoff to be discharged at lowest point, to prevent water stagnation.



Top of culvert shall be flat to ensure safe path for pedestrians to cross entrances



ABOUT THIS SERIES

"Quick Guides for Development Proposals is a series aiming to broaden and consolidate understanding of LTA's building plan regulations and processes. The guides within the series feature in-depth explanation of the principles behind specific requirements, coupled with examples of good practices & common mistakes.

Topics for each guide are carefully curated based on LTA's observations of prevailing trends. All publications are made available at LTA's corporate website.

INCLUSIVE STREETS: DESIGNING TACTILE INDICATORS FOR SAFE TRAVEL

Objectives of tactile indicators

Tactile indicators are required where footpaths intersect with drainage of developments and/or pedestrian crossings to guide the visually impaired pedestrians. The correct placement of tactile indicators for various situations allows the way for safe travel for the visually impaired pedestrians. Commonly, incorrect placement of tactile indicators can lead to hazardous situations. This quick guide helps Architects, Engineers and Builders to identify the correct placement of tactile indicators for various situations. There are also illustrations of common errors to be avoided.

Positioning tactile indicators correctly

Follow these steps to correctly position the tactile indicators:

1. Determine whether the footpath is intersecting the drainage/drainage on a straight, skewed or curved point of the drainage/drainage.
2. To help in the placement of the tactile indicators, you may draw a imaginary line parallel to the edge of the drainage/drainage.
3. Place 2 rows of tactile indicators touching this imaginary line as shown in Figure 1.
4. Ensure that the width of the tactile indicators covers the entire width of the drainage/drainage.

Figure 1 - Straight Drainage / Road

Figure 2 - Skewed

Figure 3 - Curved

Quick Guide Series for Development Proposals, brought to you by Development & Building Control Division

1.0 Objectives of Covered Linkways

The use of covered linkways plays an important role in achieving sheltered connectivity for pedestrians and commuters. Properly designed covered linkways enable weatherproof pedestrian connectivity between developments to transport routes like bus stops and train stations. Commonly, inadequate design of covered linkways can lead to inconvenient situations for the public.

2.0 Low Covered Linkway

2.1 Typical Plan Presentation for a Low Covered Linkway

Figure 1 - Typical Plan Presentation for Low Covered Linkway

Figure 2 - Sectional representation for Design Criteria

2.3 Common Mistakes in Low Covered

Figure 3 - Common Mistake: The design of the low covered linkway should be able to accommodate existing infrastructure and avoid common mistakes.

Figure 4 - Sectional representation summarizing

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Access Arrangements for Residential Developments

Objectives

Within any development, the design, position and arrangement of access points are critical. They ensure that vehicles can enter or leave the development safely, and do not adversely affect traffic along the public street to which it connects.

About this series

Since April 2020, LTA has been publishing a series of quick guides to improve the industry's understanding of LTA's building plan regulations and processes. The guides feature an in-depth explanation of the principles behind specific requirements, coupled with examples of good practices & common mistakes.

This quick guide aims to help Architects, Engineers and Builders to identify the critical design elements for the access points of residential developments, better appreciate the principles behind these requirements, and avoid making common mistakes.

Figure 1 - Sectional representation summarizing

Quick Guide Series for Development Proposals, brought to you by Development & Building Control Division

INCLUSIVE DEVELOPMENTS: DESIGNING DELIVERY SPACES WITHIN MIXED DEVELOPMENTS

1.0 Objectives of Residential Delivery Spaces in Mixed Developments

There is an increasing trend of mixed or integrated developments incorporating a residential component. Partly accelerated by the COVID situation, there is also a higher demand for food, grocery and furniture deliveries to end-consumers. Designers have to adapt and cater to these trends. Designing for delivery spaces in a pure residential development is straightforward. However, designing for delivery spaces in mixed developments is more complex.

2.0 Design Criteria for Residential Delivery Spaces in Mixed Developments

The key design criteria for delivery spaces (i.e. LUL bays) for residential component in all new mixed developments are as follows:

LTA Design Criteria in Residential Component	Criteria to meet
1.1	1.1
1.2	1.2
1.3	1.3
1.4	1.4
1.5	1.5
1.6	1.6
1.7	1.7
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